

## Session : 1 NTA Standards and Guidelines Updates

**Noel Fennelly,  
Senior Standards Manager,  
National Transport Authority**

# NTA Standards & Guidelines Updates



An Roinn Iompair  
Department of Transport



**RSTG** Annual  
Conference  
2026  
14TH - 15TH MAY | WHITE'S HOTEL, CO. WEXFORD

## Contents

- Traffic Signs Manual Updates
- NTA Typical Construction Details
- Other updates

# Traffic Signs Manual Updates



An Roinn Iompair  
Department of Transport





# Session : 1

NTA Standards and Guidelines Updates

New Regulatory Signs in Chapter 5 include:

RUS 032A School Street



Instead of this



# Session : 1

NTA Standards and Guidelines Updates

New Regulatory Signs in Chapter 5 include:

**RUS 067 Parallel/Combined Zebra Crossing**



**Complements existing Zebra Crossing Sign  
RUS 066**

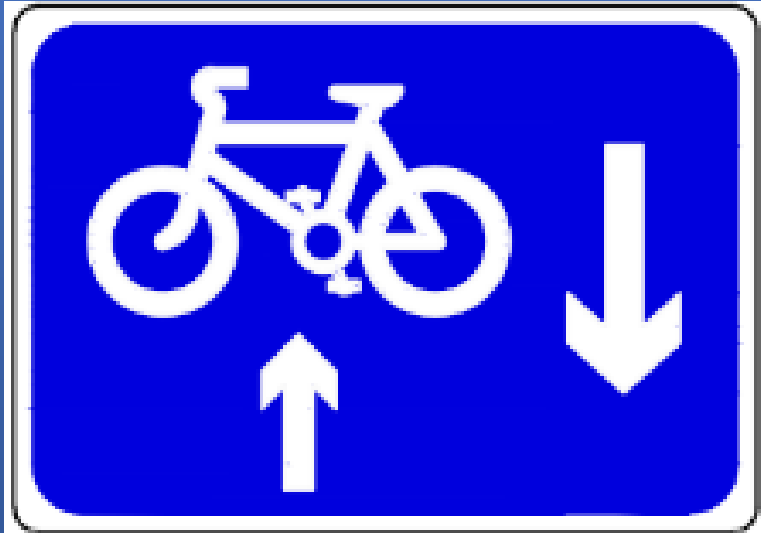


# Session : 1

NTA Standards and Guidelines Updates

New Regulatory Signs in Chapter 5 include:

## RUS 059A Contra-Flow Cycling on One-Way Street



- 30 km/h speed limit
- Low traffic streets - 1,000 AADT



# Session : 1

NTA Standards and Guidelines Updates

New Regulatory Signs in Chapter 5 include:

**RUS 070 Shared Zone**



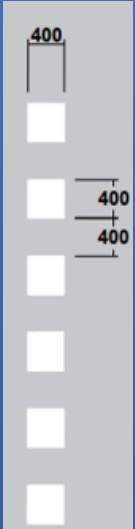
**Replaces F404**



# Session : 1 NTA Standards and Guidelines Updates

New Road Markings in Chapter 7 include:

## Elephant's Footprint Marking (RRM 038)



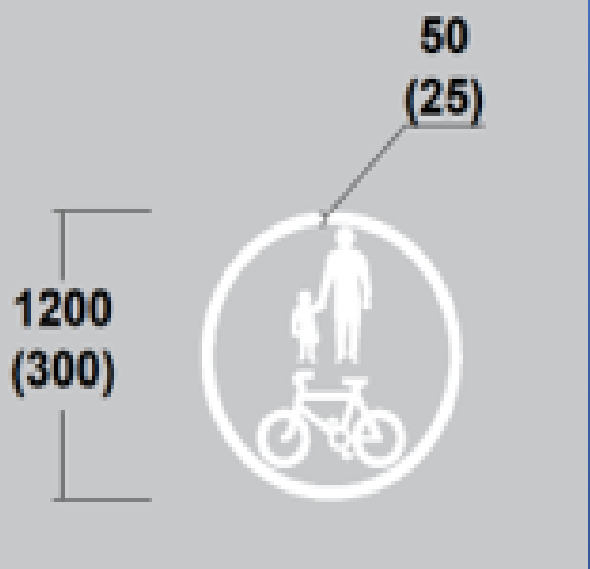
To delineate cycle tracks through junctions and crossings



# Session : 1 NTA Standards and Guidelines Updates

New Road Markings in Chapter 7 include:

## RRM 037 Non-Segregated Shared Track Symbol



NB: Matches Upright Sign RUS 009

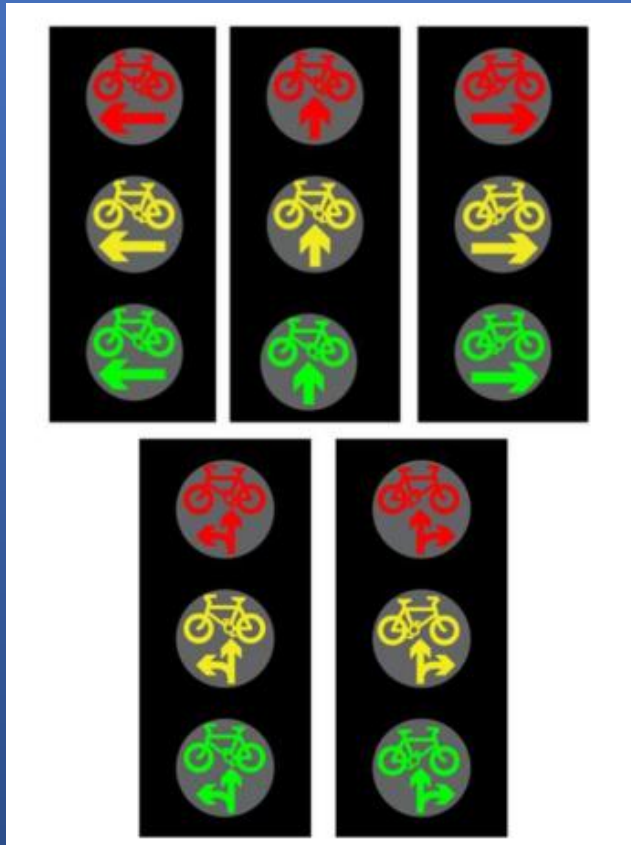


# Session : 1

NTA Standards and Guidelines Updates

Chapter 9 Traffic Signals updates include:

## Low-level Directional Cycle Signals (Variants of RTS 007)



Can be used to control separate cycle streams at signalised junctions or mid-block crossing



# NTA Typical Construction Details

# Session : 1

## NTA Standards and Guidelines Updates

- First Batch published January 2026. Second batch due to be published shortly.
- Focus on urban infrastructure.
- TCDs are there to assist and promote more uniform approaches to the design and construction of some common built elements including footways, kerbs, cycle facilities and drainage systems
- NB: It remains the responsibility of designers, contractors and road authorities to choose appropriate construction details (including TII standard construction details or bespoke details as appropriate) for any given project



**Series NTA 500: Drainage and Ducts**



**Series NTA-700: Pavement**



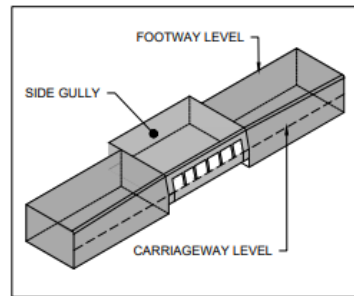
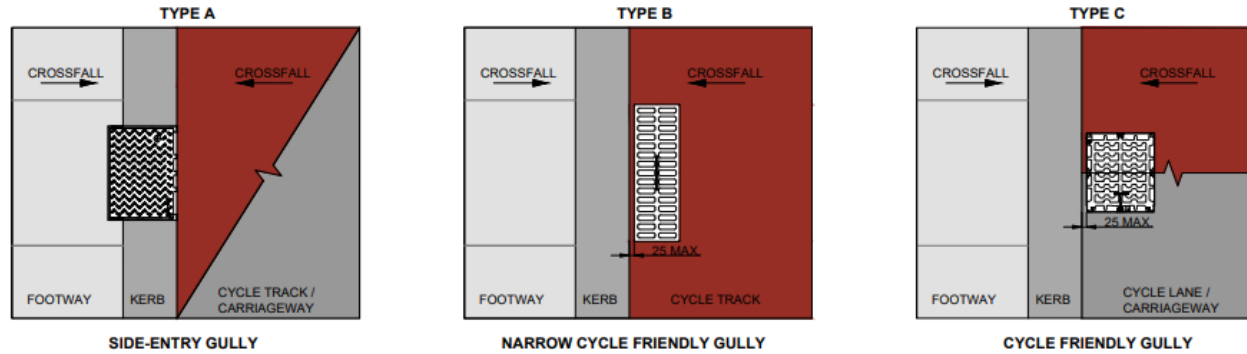
**Series NTA-1100: Kerbs, Footways and Paved Areas**

<https://www.nationaltransport.ie/publications/nta-typical-construction-details/>

# Session : 1 NTA Standards and Guidelines Updates

Drawing Number	Drawing Title
<b>Series 500: Drainage and Ducts</b>	
NTA-TCD-500-001	Active Travel Gully Gratings
NTA-TCD-500-002	Linear Drainage Systems
NTA-TCD-500-003	Combined Kerb and Drainage System
NTA-TCD-500-004	Gully relocation (Rapid Build Cycle Track)
NTA-TCD-500-005	Typical details for traffic signal Chambers
NTA-TCD-500-006	Typical details for traffic signal ducting
NTA-TCD-500-007	Ducting Arrangement (Protected Junction)
NTA-TCD-500-008	Ducting Arrangement (Protected Junction - CYCLOPS Layout)
NTA-TCD-500-009	Ducting Arrangement (Protected Junction - Full Signal Control)
NTA-TCD-500-010	Ducting Arrangement (Signalised Pedestrian Crossing - Mid-block)
NTA-TCD-500-011	Ducting Arrangement (Parallel Zebra Crossing)
NTA-TCD-500-012	Ducting Arrangement (Combined Zebra Crossing)
NTA-TCD-500-013	Ducting Arrangement (Signalised Parallel Crossing)
NTA-TCD-500-014	Ducting Arrangement (Toucan Crossing)
NTA-TCD-500-015	Ducting arrangement (Signalised cycle crossing)

# Session : 1 NTA Standards and Guidelines Updates



- NOTES:**
1. REFER TO TII CC-SCD-0510 FOR DETAILS OF PRECAST GULLY CONSTRUCTION.
  2. REFER TO TII CC-SCD-0511 FOR DETAILS OF IN-SITU AND BLOCKWORK GULLY CONSTRUCTION.
  3. GULLY GRATING TO BE CONSTRUCTED FLUSH WITH CYCLING SURFACE (TYPICALLY 0mm TO -5mm).
  4. GULLY SLOTS MUST BE AT RIGHT ANGLES TO CYCLING DIRECTION.
  5. GULLY TYPE TO BE AGREED WITH THE CONTRACTING AUTHORITY.
  6. DESIGN PREFERENCE IS FOR FOOTWAY / CYCLE TRACK TO DRAIN TO CARRIAGEWAY TO REMOVE THE NEED FOR GULLY GRATINGS IN PEDESTRIAN AREAS OR CYCLEWAYS.
  7. GULLY TYPE A IS DESIRABLE ON NEW ACTIVE TRAVEL SCHEMES.
  8. GULLY GRATING TO COMPLY WITH IS EN 124.
  9. ALL GULLY GRATES AND FRAMES SHOULD BE MINIMUM GRADE D400 IN CARRIAGEWAY (INCLUDING AREAS OF FOOTWAY / VERGE WITH POTENTIAL FOR VEHICLE OVERRUN) AND C250 IN VERGE / FOOTWAY, UNLESS OTHERWISE AGREED WITH THE CONTRACTING AUTHORITY.

NOT TO SCALE



An Roinn Iompair  
Department of Transport



## TYPICAL CONSTRUCTION DETAILS

Series:

NTA-500: DRAINAGE AND DUCTS

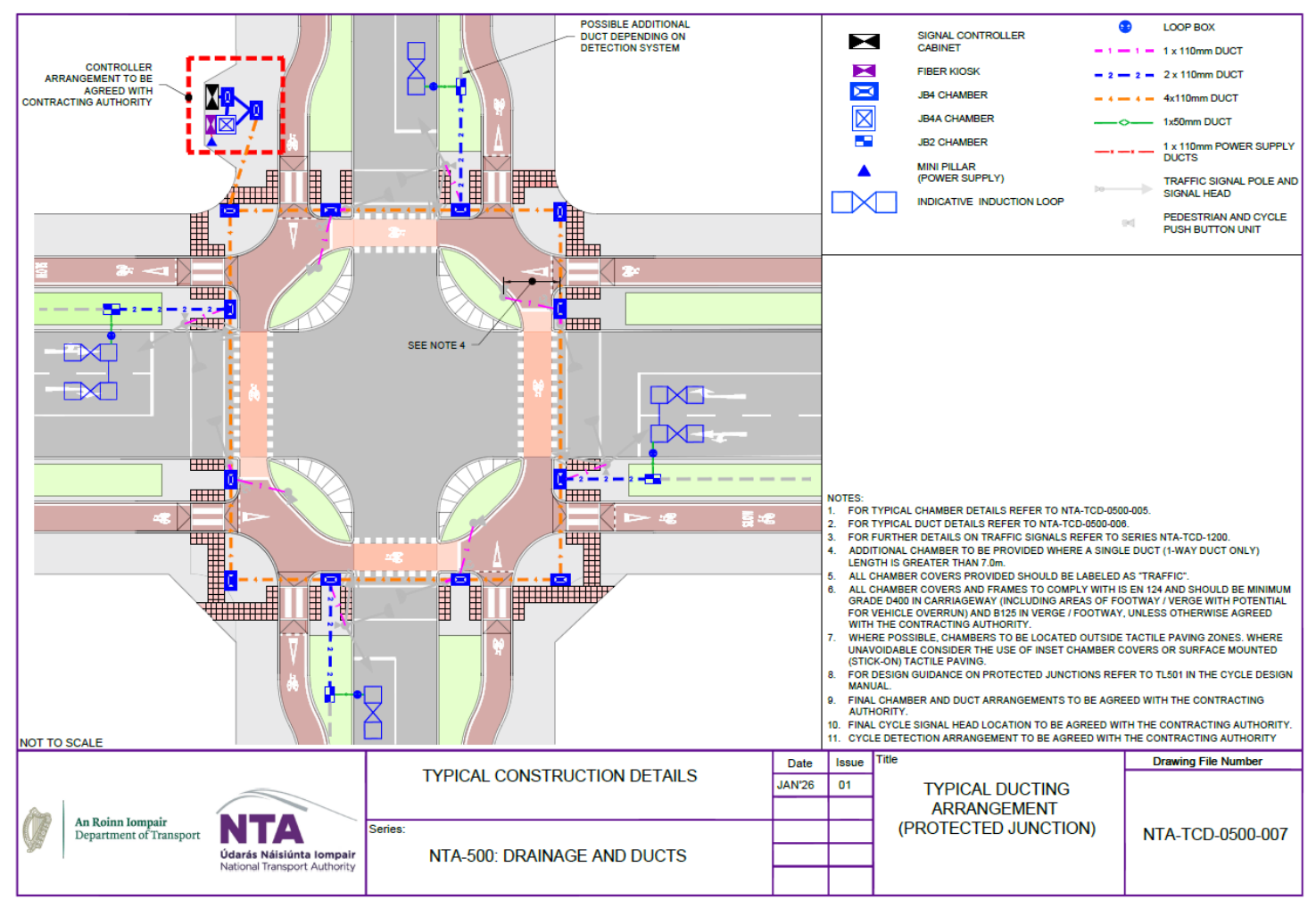
Date	Issue
JAN'26	01

## ACTIVE TRAVEL GULLY GRATINGS

Drawing File Number

NTA-TCD-0500-001

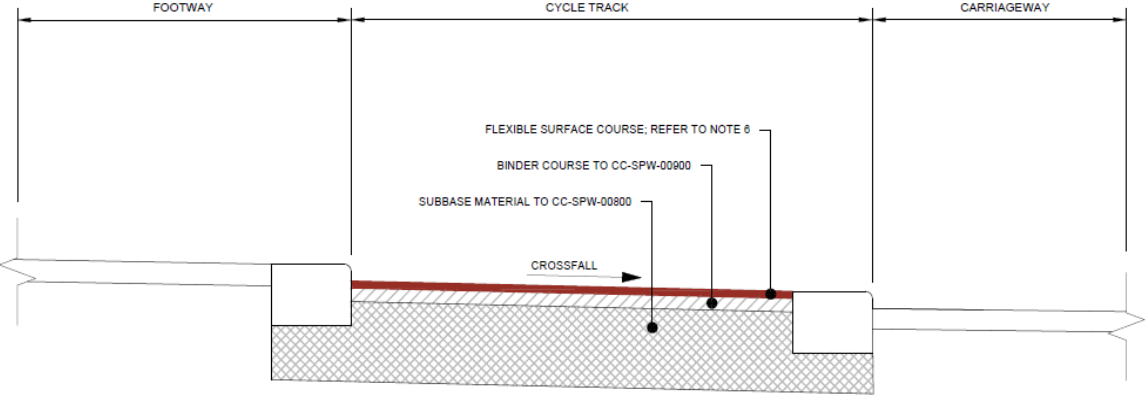
# Session : 1 NTA Standards and Guidelines Updates



# Session : 1 NTA Standards and Guidelines Updates



Series NTA-700: Pavement	
NTA-TCD-700-001	Standard Cycle Track Pavement
NTA-TCD-700-002	Stepped Cycle Track Pavement
NTA-TCD-700-003	Protected Cycle Lane Pavement
NTA-TCD-700-004	Pavements for Shared Greenways and Active Travel Facilities
NTA-TCD-700-005	Raised Pedestrian Crossing (Flexible Pavement)
NTA-TCD-700-006	Raised Pedestrian Crossing - Sinusoidal (Flexible Pavement)
NTA-TCD-700-007	Side Road Entry Ramp (Flexible Pavement)
NTA-TCD-700-008	Speed Cushion (Flexible Pavement)
NTA-TCD-700-009	Speed Ramp - Flat Top (Flexible Pavement)
NTA-TCD-700-010	Speed Ramp - Round Top (Flexible Pavement)
NTA-TCD-700-011	Central raised strip on Mixed Traffic Routes (imprinted Asphalt)
NTA-TCD-700-012	Central raised strip on Mixed Traffic Routes (Block Paving / Stone Sets)
NTA-TCD-700-013	Corner Overrun Area
NTA-TCD-700-014	Carriageway Widening & Narrowing Detail
NTA-TCD-700-015	Ironwork Mastic Surround

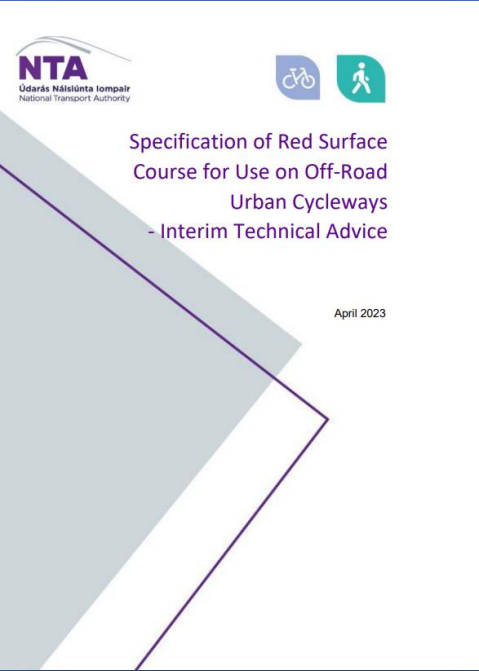
# Session : 1 NTA Standards and Guidelines Updates



- NOTES:
1. FOR APPROVED KERB PROFILES REFER TO NTA-TCD-1100-001 TO 005.
  2. FOR KERB CONSTRUCTION REFER TO NTA-TCD-1100-011.
  3. FOR FOOTWAY TYPES REFER TO NTA-TCD-1100-015 TO 018.
  4. MACHINE LAID SUBBASE IS RECOMMENDED TO IMPROVE FINAL RIDE QUALITY OF CYCLE TRACK.
  5. CONSIDERATION TO BE GIVEN TO KERB HAUNCH WHERE VEHICLES MAY BE PRONE TO MOUNT KERB ONTO CYCLE TRACK.
  6. FOR FURTHER DETAIL ON CYCLE TRACK PAVEMENT SPECIFICATION REFER TO THE LATEST VERSION OF NTA "SPECIFICATION OF RED SURFACE COURSE FOR USE ON OFF-ROAD URBAN CYCLEWAYS" AND SECTION 5.2 OF THE CDM.

NOT TO SCALE

 	TYPICAL CONSTRUCTION DETAILS	Date	Issue	Title	Drawing File Number
	Series: NTA-700: PAVEMENT	JAN'26	01	STEPPED CYCLE TRACK PAVEMENT	NTA-TCD-0700-002



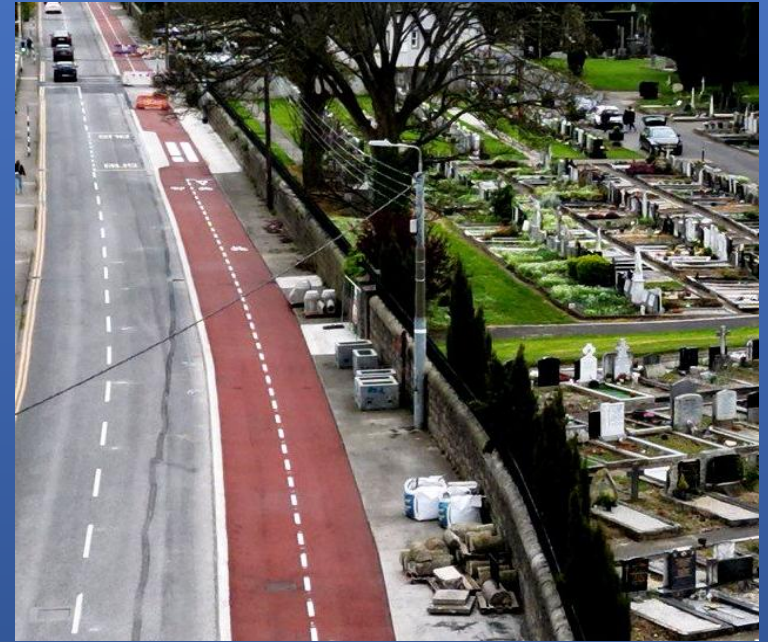
Black

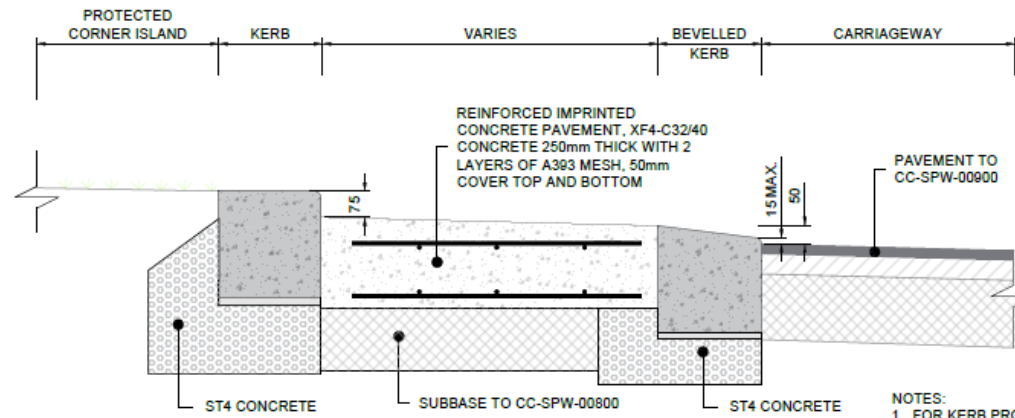
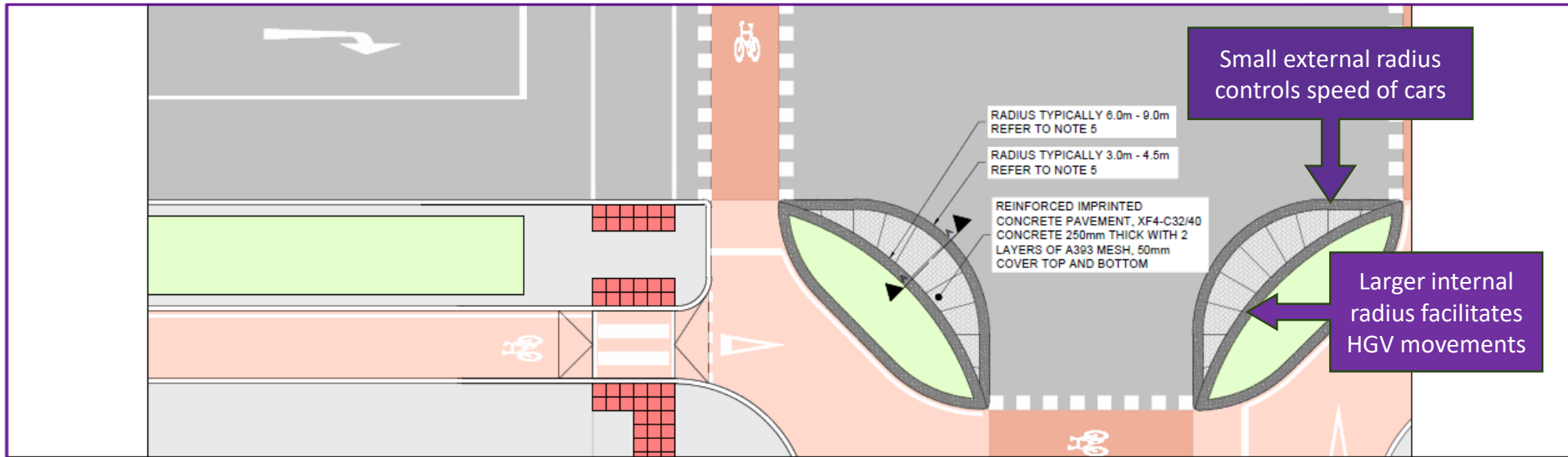
VS

Red



# Session : 1 NTA Standards and Guidelines Updates







SECTION A-A

- NOTES:
1. FOR KERB PROFILES REFER TO NTA-TCD-1100-001 TO 005.
  2. FOR KERB CONSTRUCTION REFER TO NTA-TCD-1100-011.
  3. FOR FOOTWAY TYPES REFER TO NTA-TCD-1100-015 TO 018.
  4. PAVEMENT DESIGN TO BE AGREED WITH THE CONTRACTING AUTHORITY.
  5. FOR FURTHER INFORMATION ON CORNER RADIUS REFER TO DMURS SECTION 4.3.3 AND ADVICE NOTE 6.
  6. ALL DIMENSIONS IN MILLIMETRES U.N.O.

NOT TO SCALE

 <p>An Roinn Iompair Department of Transport</p>  <p><b>NTA</b> Údarás Náisiúnta Iompair National Transport Authority</p>	<p>TYPICAL CONSTRUCTION DETAILS</p> <p>Series:</p> <p>NTA-700: PAVEMENT</p>	<p>Date</p> <p>JAN'26</p>	<p>Issue</p> <p>01</p>	<p>Title</p> <p>CORNER OVERRUN AREA</p>	<p>Drawing File Number</p> <p>NTA-TCD-0700-013</p>

# Session : 1

NTA Standards and Guidelines Updates

## Corner overrun examples (Ireland)



# Session : 1

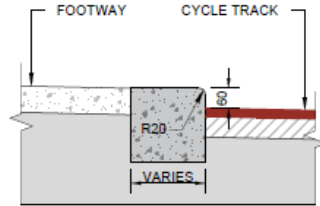
NTA Standards and Guidelines Updates

## Corner overrun examples (Netherlands)

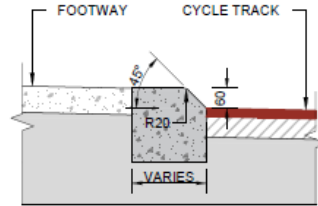


# Session : 1 NTA Standards and Guidelines Updates

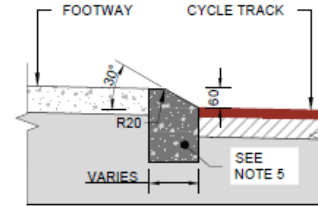
Series NTA-1100: Kerbs, Footways and Paved Areas	
NTA-TCD-1100-001	Cycle Track Kerb Profiles (Footway Interface)
NTA-TCD-1100-002	Cycle Track Kerb Profiles (Carriageway Interface)
NTA-TCD-1100-003	Standard Cycle Track Kerb Profiles
NTA-TCD-1100-004	Stepped Cycle Track Kerb Profiles
NTA-TCD-1100-005	Protected Cycle Lane Kerb Profiles (Permanent)
NTA-TCD-1100-006	Continuous Footway Crossing (Partial Setback - With Verge)
NTA-TCD-1100-007	Continuous Footway Crossing (Partial Setback - No Verge)
NTA-TCD-1100-008	Continuous Footway Crossing (No Setback)
NTA-TCD-1100-009	Driveway entry detail (Cycle track)
NTA-TCD-1100-010	Driveway entry detail (Cycle Lane)
NTA-TCD-1100-011	Kerb Construction
NTA-TCD-1100-012	Bus Stop Kerb
NTA-TCD-1100-013	Edge Kerb Construction
NTA-TCD-1100-014	Delineator Strip Construction
NTA-TCD-1100-015	In-situ concrete Footway Construction
NTA-TCD-1100-016	Flexible Footway construction
NTA-TCD-1100-017	Paved Footway construction (Slabs / Flags)
NTA-TCD-1100-018	Tree Root Protection (Shallow Footway Construction)
NTA-TCD-1100-019	In-situ Concrete Segregation Island
NTA-TCD-1100-020	Precast kerb and Paved infill Segregation Island
NTA-TCD-1100-021	Precast Segregation Island
NTA-TCD-1100-022	Standard Cycle Track Kerb Profile G Drainage Gap



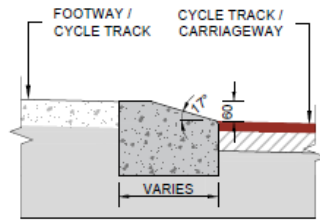
KERB PROFILE A



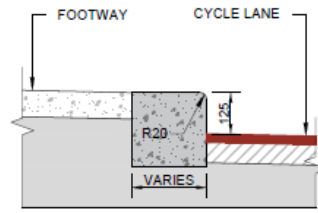
KERB PROFILE B  
KERB FACE SLOPE: 45 DEGREES



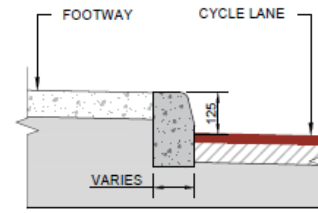
KERB PROFILE C  
KERB FACE SLOPE: 30 DEGREES



KERB PROFILE D



KERB PROFILE E



KERB PROFILE F

NOTES:

1. FOR FURTHER DETAILS ON CYCLE LINKS REFER TO SECTION 2.4 AND 4.2 OF THE CDM.
2. FOR CYCLE TRACK PAVEMENT DETAILS REFER TO NTA-TCD-0700-001 TO 004.
3. FOR FOOTWAY DETAILS REFER TO NTA-TCD-1100-015 TO 018.
4. FOR KERB CONSTRUCTION REFER TO NTA-TCD-1100-011.
5. FOR KERB PROFILE C, ENSURE SUFFICIENT VISUAL CONTRAST BETWEEN FOOTWAY SURFACE AND KERB. THE FOOTWAY AND KERB FINISH SHOULD NOT BE FROM THE SAME COLOUR PALETTE.
6. KERB PROFILE D CAN BE UTILISED AS AN ACCESS KERB. FOR ACCESS KERB DETAILS REFER TO NTA-TCD-1100-006 TO 010.
7. KERBS CAN BE CONSTRUCTED IN PRECAST CONCRETE, EXTRUDED CONCRETE, NATURAL STONE OR CAST IN-SITU CONCRETE AND KERB WIDTHS WILL VARY DEPENDING ON CONSTRUCTION METHODOLOGY.
8. ALL DIMENSIONS IN MILLIMETRES U.N.O.

NOT TO SCALE



An Roinn Iompair  
Department of Transport



TYPICAL CONSTRUCTION DETAILS

Series:

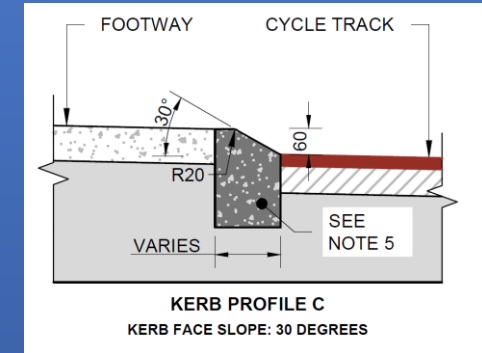
NTA-1100: KERBS, FOOTWAYS AND PAVED AREAS

Date	Issue	Title	Drawing File Number
JAN'26	01	CYCLE TRACK KERB PROFILES (FOOTWAY INTERFACE)	NTA-TCD-1100-001

# Session : 1

NTA Standards and Guidelines Updates

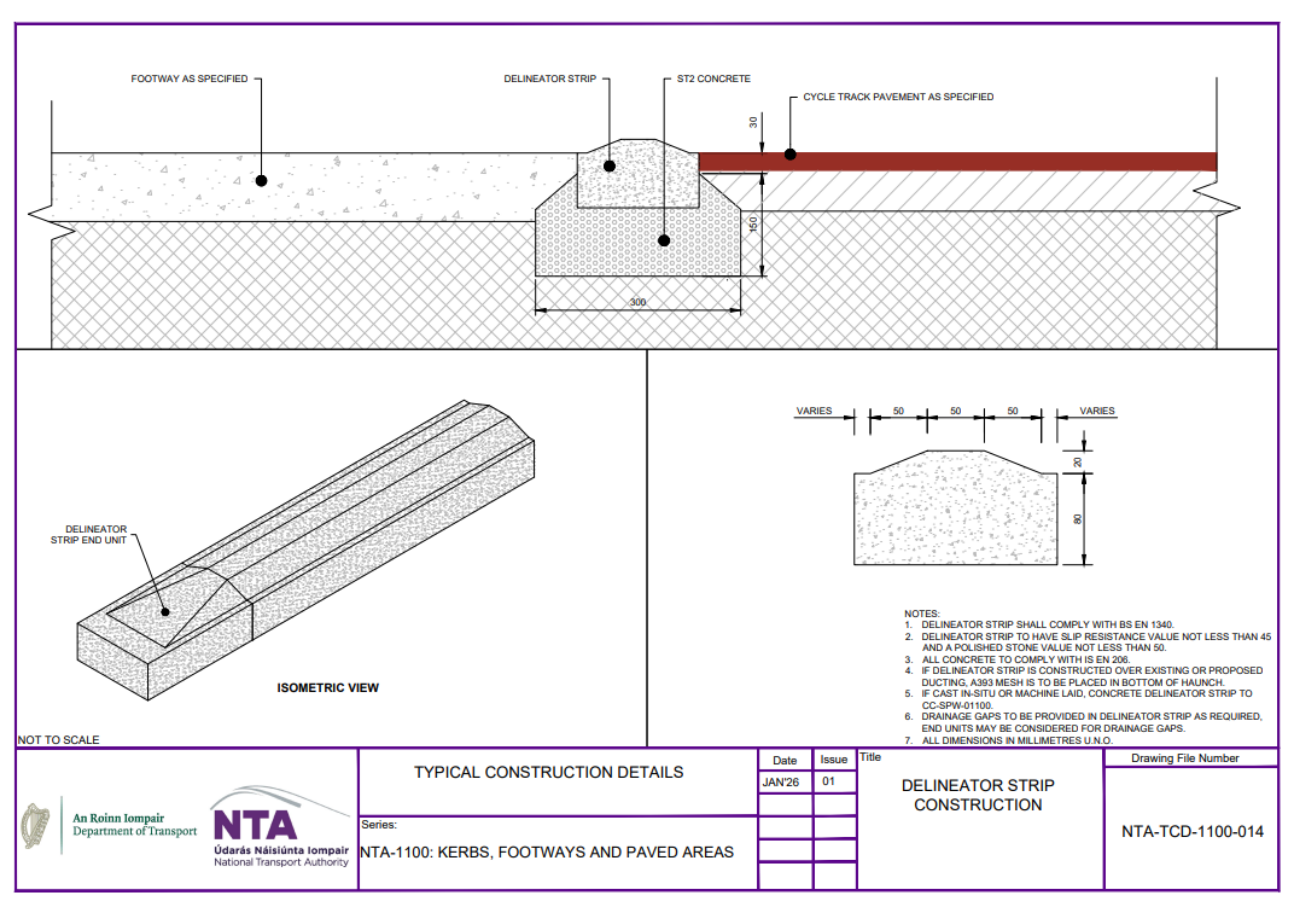
## Kerb Profile C (30 degree)



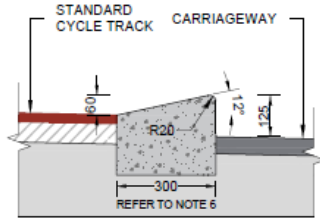
- More forgiving for cyclists
- Improves effective width of cycle track
- NB: Ensure sufficient visual contrast between footway surface and kerb

# Session : 1 NTA Standards and Guidelines Updates

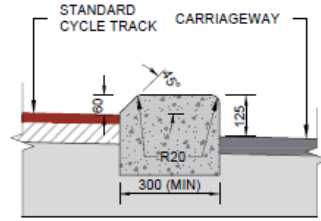
## Delineator Strip



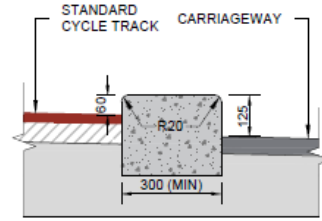
- Profile as per UK Tactile Guidance Document
- Where vertical segregation or horizontal separation isn't possible
- May reduce the potential for trip hazards in pedestrian rich environments



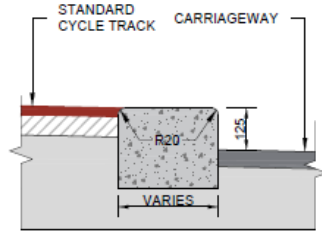
**KERB PROFILE G**  
KERB FACE SLOPE: 12 DEGREES



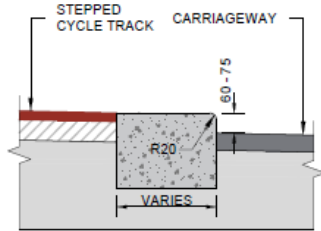
**KERB PROFILE H**  
KERB FACE SLOPE: 45 DEGREES



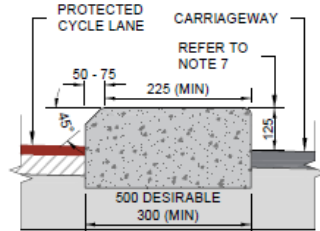
**KERB PROFILE J**



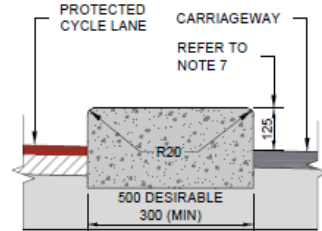
**KERB PROFILE K**



**KERB PROFILE L**

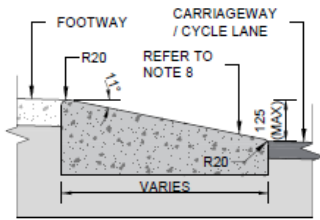


**KERB PROFILE M**  
KERB FACE SLOPE: 45 DEGREES

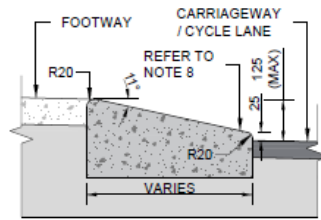


**KERB PROFILE N**

**KERBS AT CYCLE TRACK / LANE - CARRIAGEWAY INTERFACE**

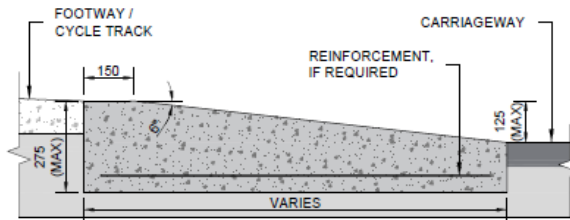


**KERB PROFILE P**  
KERB FACE SLOPE: 11 DEGREES (1:5)



**KERB PROFILE Q**  
KERB FACE SLOPE: 11 DEGREES (1:5)

**ENTRY KERBS AT DRIVEWAYS**





**KERB PROFILE R**  
KERB FACE SLOPE: 5.7 DEGREES (1:10)

**ENTRY KERB AT SIDE ROADS AND CONTINUOUS CROSSINGS**

**NOTES:**

1. FOR FURTHER DETAILS ON CYCLE LINKS REFER TO SECTION 2.4 AND 4.2 OF THE CDM.
2. FOR CYCLE TRACK PAVEMENT DETAILS REFER TO NTA-TCD-0700-001 TO 004.
3. FOR FOOTWAY DETAILS REFER TO NTA-TCD-1100-015 TO 018.
4. FOR KERB CONSTRUCTION REFER TO NTA-TCD-1100-011.
5. FOR ACCESS KERB DETAILS REFER TO NTA-TCD-1100-006 TO 010.
6. 300mm DESIRABLE MINIMUM WIDTH FOR KERB PROFILE G WITH ABSOLUTE MINIMUM OF 250mm. WHERE A 250mm WIDE KERB IS PROPOSED, A DESIGNER RISK ASSESSMENT SHOULD BE COMPLETED AND A DEPARTURE OR DEROGATION FROM STANDARD SHOULD BE SOUGHT AND APPROVED IN ACCORDANCE WITH DEPARTMENT OF TRANSPORT 'NGS CIRCULAR 2 OF 2022'.
7. FOR KERB TYPES M AND N, A REDUCED UPSTAND HEIGHT MAY BE IMPLEMENTED ON DESIGNATED EMERGENCY SERVICE ROUTES. WHERE A REDUCED UPSTAND IS PROPOSED, A DESIGNER RISK ASSESSMENT SHOULD BE COMPLETED AND A DEPARTURE OR DEROGATION FROM STANDARD SHOULD BE SOUGHT AND APPROVED IN ACCORDANCE WITH DEPARTMENT OF TRANSPORT 'NGS CIRCULAR 2 OF 2022'.
8. DESIGN PREFERENCE IS FOR KERB PROFILE P AT DRIVEWAYS, WITH KERB PROFILE Q BEING USED WHERE FOOTWAY WIDTH IS LIMITED.
9. KERBS CAN BE CONSTRUCTED IN PRECAST CONCRETE, EXTRUDED CONCRETE, NATURAL STONE OR CAST IN-SITU CONCRETE AND KERB WIDTHS WILL VARY DEPENDING ON CONSTRUCTION METHODOLOGY.
10. ALL DIMENSIONS IN MILLIMETRES U.N.O.

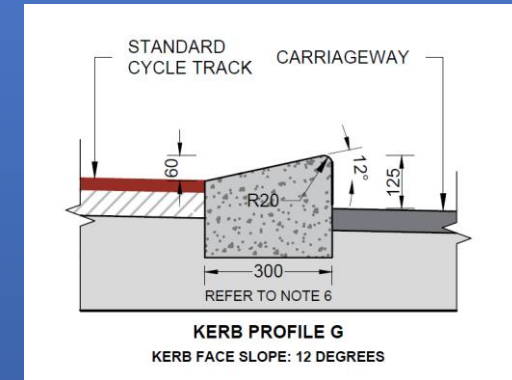
NOT TO SCALE

 	<b>TYPICAL CONSTRUCTION DETAILS</b>		Date	Issue	<b>CYCLE TRACK KERB PROFILES (CARRIAGEWAY INTERFACE)</b>	Drawing File Number
			JAN'26	01		
Series:						
NTA-1100: KERBS, FOOTWAYS AND PAVED AREAS						

# Session : 1

## NTA Standards and Guidelines Updates

### Kerb Profile G (a.k.a. Castletymon kerb)

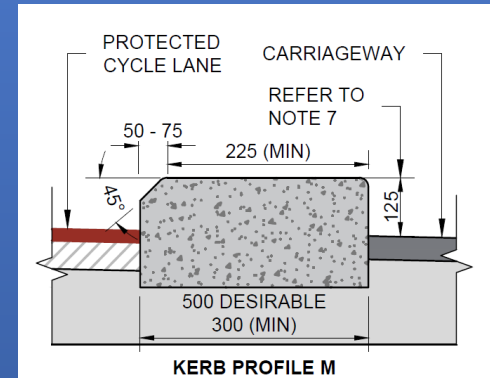


- Full height kerb protection from traffic
- Cycle track at intermediate level, 60mm above carriageway
- No upstand on cycle track side
- Increases effective width of cycle track

# Session : 1

NTA Standards and Guidelines Updates

## Kerb Profile M



- 500mm width desirable (300mm absolute minimum)
- 125mm height recommended

# Other Updates



An Roinn Iompair  
Department of Transport



# Session : 1 NTA Standards and Guidelines Updates



- First update to CDM due in Q2 2026
- Significant update to guidance on Island Bus Stops, informed by discussions with stakeholders
- Other minor updates to align with TSM etc.
- Subsequent update likely by end of 2026

# Session : 1

## NTA Standards and Guidelines Updates

### Updated guidance on Island Bus Stops

- Changes informed by detailed engagement with stakeholders and disability groups
  - New approach – recommended core and optional features
  - Existing layouts updated with new features
  - New layouts for reduced-width islands, two-way cycle tracks and bus stops adjacent to shared paths
- Aiming to achieve minimum island width of 2m wherever possible. Islands less than 2 metres will likely require a departure





## 5.0. Interventions

This section outlines a comprehensive suite of traffic calming and road safety interventions. These interventions are most effective when a range of measures are implemented together rather than as standalone solutions. Based on the outcomes of community engagement sessions, the most appropriate interventions that align with the project's objectives and community needs will be selected for implementation, while others may be reserved for future opportunities.

The interventions are organised into four thematic categories illustrated below, each essential to the overall success of the scheme:

### Traffic Reduction Measures:

These are the primary interventions aimed at eliminating non-local traffic from the area and should be the first consideration after defining the boundaries of the study area.

### Traffic Calming Measures:

Following traffic reduction, these measures aim to moderate the speed of any remaining traffic within the neighbourhood, ensuring a safer environment for all users.

### Ancillary Measures:

This category includes essential interventions that do not fit neatly into the other categories but are vital to the comprehensive development and success of the neighbourhood. These measures should not be viewed as "bonus" measures or "add-ons" but as integral components that support the overarching goals of the scheme.

### Placemaking Interventions:

With reduced and calmed traffic, the focus shifts to placemaking efforts that enhance the public realm, emphasising pedestrian priorities and creating spaces for social interaction, play and relaxation.



# Session : 1 NTA Standards and Guidelines Updates

**Noel Fennelly, Senior Standards Manager, National Transport Authority**

# THANK YOU

Email queries to: [TechnicalStandards@nationaltransport.ie](mailto:TechnicalStandards@nationaltransport.ie)



An Roinn Iompair  
Department of Transport



**RSTG** Annual  
Conference  
2026  
14TH - 15TH MAY | WHITE'S HOTEL, CO. WEXFORD

Join the Q&A session at **Slido.com**  
and enter 2063208 or via the **QR**  
**Code.**

**Questions must include name  
and associated Local Authority  
to be considered by the panel.**

